

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HARBORS

ADDENDUM NO. 1

FOR

**FENDER REPAIRS AT PIER 3,
HILO HARBOR, HAWAII**

JOB S50208

May 25, 2023

This Addendum shall make the following amendments to the bid documents:

I. SPECIFICATIONS

A. SPECIAL PROVISIONS

1. Delete Special Provisions SP-2 in its entirety and replace with the attached Special Provisions SP-2 dated r5/25/2023.

B. ARTICLE X – PROJECT DESCRIPTION

1. Delete Article X - Project Description, page 10-7 in its entirety and replace with the attached Article X - Project Description, page 10-7 dated r5/25/2023.

C. ARTICLE XII – FENDER SYSTEM REPAIRS

1. Delete Article XII – Fender System Repairs, pages 12-1 through 12-3 in its entirety and replace with the attached Article XII – Fender System Repairs, pages 12-1 through 12-3 dated r5/25/2023.

D. PROPOSAL

1. Delete Proposal pages P7 and P8 in its entirety and replace with the attached Proposal pages P7 and P8 dated r5/25/2023.

II. PLANS

A. PLAN

1. Delete Plan pages 2 and 3 in its entirety and replace with the attached Plan pages 2 and 3 dated Rev. 5/25/2023.

III. PRE-BID MEETING

The pre-bid meeting was held on May 19, 2023, at 9:00 a.m. The pre-bid meeting minutes are attached for your information.

Kindly acknowledge receipt of this Addendum No. 1 by recording the date of its receipt in the space provided therefore on page P-4 of the PROPOSAL.



DREANALEE K. KALILI
Deputy Director
Department of Transportation, Harbors

2. 2.8 Preparation and Delivery of Bid is amended as follows: Last paragraph (lines 189 to 192) shall be replaced with the following:

“The bidder shall submit the proposal in HiePRO. The proposal shall be UPLOADED to HiePRO prior to the bid opening date and time. Proposals received after said due date and time shall not be considered. Original (wet ink) proposal documents are not required to be submitted. The award will be made based on proposals uploaded in HiePRO. Any and all other additional documents explicitly designated and labeled as CONFIDENTIAL or PROPRIETARY shall be UPLOADED SEPARATELY to HiePRO.”

3. 2.11 Bid Security is amended by deleting (a) and replacing it with:

“(a) Unless directed otherwise in the invitation for bids, each bid shall be accompanied by bid security which is intended to protect the Department against the failure or refusal of a bidder to execute the contract for the work bid or to supply the required performance and payment bonds. Bid security shall be in an amount equal to at least five percent of the base bid and additive alternates. Bid security shall be in one of the following forms:

- (1) A deposit of legal tender;
- (2) A valid surety bid bond, underwritten by a company licensed to issue bonds in the State of Hawaii; or
- (3) A certificate of deposit; credit union share certificate; or cashier’s, treasurer’s, teller’s, or official check drawn by or a certified check accepted by a bank, savings institution, or credit union insured by the Federal Deposit Insurance Corporation or the National Credit Union Administration and payable at sight or unconditionally assigned to the Department. These instruments may be utilized only to a maximum of \$100,000.00. If the required amount totals over \$100,000.00, more than one instrument not exceeding \$100,000.00 each and issued by different financial institutions shall be accepted.
- (4) Proposal Guaranty listed in (1) and (3) shall be in its original form, and shall be received at the Contracts Office, Department of Transportation, Aliiaimoku Hale, 869 Punchbowl Street, Room 105, Honolulu, Hawaii 96813, before the bid deadline.”

4. 2.12 Pre-Opening Modification or Withdrawal of Bids is amended by deleting 2.12 Pre-Opening Modification or Withdrawal of Bids in its entirety and replacing it with the following:

new eye bolt assemblies including new eyebolts, anchoring adhesive, and new metal appurtenances; and any other incidental items and work required to complete this item.

- Item 2 Concrete Haunch Spall Repair - Payment shall be made at the unit price bid in the Proposal Schedule. Such payment shall include removal and disposal of the existing reinforced concrete haunch; installing new reinforcing steel; preparing the concrete surface; forming and installing new concrete mortar; and other incidental work required to complete this item.
- Item 3 Concrete Haunch Full Repair - Payment shall be made at the unit price bid in the Proposal Schedule. Such payment shall include removal and disposal of the entire existing reinforced concrete haunch; installing new reinforcing steel; preparing the concrete surface; forming and installing new concrete; and other incidental work required to complete this item.
- Item 4 Replacement Tire Bumpers - Payment shall be made at the unit price bid in the Proposal Schedule. Such payment shall include furnishing and installing new replacement tire bumpers and any other incidental work required to complete this item.
- Item 5 New Tire Bumper Hardware - Payment shall be made at the unit price bid in the Proposal Schedule. Such payment shall include removal and disposal of the existing rubber tire fender bumper chains and shackles from State property in a lawful manner; installing new rubber tire fender bumper chains, shackles, grommets, and other metal appurtenances necessary; and any other incidental work required to complete this item.

ARTICLE XII – FENDER SYSTEM REPAIRS

12.1 GENERAL – The work to be done under this Article consists of repairing the entire rubber tire fender system for Pier 3 at Hilo Harbor. The work includes, but is not limited to the following:

- A. Remove existing fender system, including tire bumpers and fastenings, vertical lagging, rubbing blocks, upper wales, lower wale and Type “I”, “II”, and “III” bolts.
- B. Furnish and install new Type “I”, “II”, and “III” bolts.
- C. Furnish and install new 14-inch by 14-inch upper wales of the various lengths.
- D. Furnish and install new 4-inch by 6-inch rubbing blocks with 3-inch by 4-inch by 3/8-inch steel angle at all new tire bumper assemblies.
- E. Furnish and install new spacer blocking as required.
- F. Furnish and install new vertical lagging as required.
- G. Reinstall previously removed tire bumpers using new chains, shackles, grommets, and fasteners.
- H. Furnish and install new tire bumper assemblies where shown on the plans including new chains, shackles, grommets, and eye bolts.
- I. Drill various sized holes in tires for installation of new tire grommets and 3-inch drain holes.
- J. Furnish and install new miscellaneous fastening hardware such as boat spikes, lag screws, wire nails, etc., as required.

12.2 OBSTRUCTIONS TO NAVIGATION – Should the Contractor, during the progress of work, lose, dump, sink, or misplace any material, plant, machinery, or equipment in harbor waters, and if in the opinion of the State Harbor Master it becomes a danger or obstruction of navigation, the Contractor shall recover and remove immediately.

12.3 MATERIALS

- A. Timber - All new timber, including timber wales, rubbing blocks, spacer blocks, and vertical lagging, shall be rough select structural grade and subject to **American Wood Protection Association Standard U1-17 in category UC5C Waters. Treatment of coastal Douglas fir and plywood solid sawn material for**

ammoniacal copper zinc arsenate treatment is to have a retention of 2.5 pounds per cubic foot (pcf) and for creosote treatment is to have a retention of 25 pcf. Timber shall be Douglas fir, straight, free from splits, checks or other defects, which may weaken the member structurally. The Contractor shall furnish a certificate from the supplier certifying to the treatment of the lumber.

All surfaces of pressure coat tar creosote treated lumber cut after treatment shall be treated with “Jasco Termin-8” or approved equal.

- B. Rubbing blocks at tire bumper assemblies shall be 6-inch by 4-inch Douglas fir rough select construction grade, sound, dry, undamaged and free of cracks, splits, checks and other defects. The Contractor shall furnish a certificate from the supplier certifying to the treatment of the lumber.
- C. Spacer blocks shall be marine, exterior grade, Douglas Fir Plywood Association B/B plywood, or approved equal, and shall be treated as specified above for rubbing blocks. Finished thickness may be built up of two 3/4-inch-thick pieces. **All plywood shall meet the U.S. Product Standard PS-1 and glued only with waterproof glue. The Contractor shall furnish a certificate from the supplier certifying to the treatment of the lumber.**
- D. Miscellaneous Timber Fasteners such as boat spikes (button head), nails, lag screws, and common wire nails shall be hot dip galvanized.
- E. Eye Bolts – 3/4-inch Crosby Shoulder Nut Eye Bolt with working in-line working load limit of 7,200 pounds or approved equal. Use teal colored Simpson Strong-Tie SET-XP high strength anchoring adhesive epoxy when installing eyebolts.
- F. Ogee Washers shall be hot dip galvanized, ogee pattern, cast iron. Size to fit new stud bolts.
- G. Type “I”, “II”, and “III” Bolts - Shall be fabricated from round bars conforming to American Society for Testing and Materials (ASTM) A-36 standard and threaded a minimum of three inches at both ends. The bolts shall be furnished with cast iron ogee washers and hex nuts as required. Nuts shall conform to ASTM A-307. Washers and nuts shall be hot dip galvanized. The bolts shall be hot dip galvanized after fabrication.
- H. Chain for hanging tires shall be hot dip galvanized dock fender (mooring) chain, sized as shown on the plans. For the 1/2-inch dock fender chain, the minimum working load limit shall be 9,000 pounds. Existing chain may be accepted for use in place of new chain and shall be individually inspected and approved by the

Construction Engineer or Harbor Agent prior to use in the newly repaired fender system. It is the Contractor's responsibility to verify the length of chain necessary to completely repair the fender system, with any additional chain placed in a location deemed appropriate by the Construction Engineer and Harbor Agent for storage and future use.

- I. Shackles shall be forged, hot dip galvanized steel, anchor shackle with bolt, nut, and stainless-steel cotter pin. Five-eighths-inch anchor shackle shall have a minimum working load limit of three tons, and 7/8-inch anchor shackle shall have a minimum working load limit of 6.5 tons.
- J. Rubbing Angles – Steel rubbing angles shall be fabricated as shown on the plans and the structural steel used shall conform to ASTM A-36 and shall be hot dip galvanized in accordance with ASTM A-123.
- K. Tire Grommets – Grommet assemblies shall be fabricated as shown on the plans and the structural steel used shall conform to ASTM A-36 and shall be hot dip galvanized in accordance with ASTM A-123. All welding shall conform to the requirements of the American Welding Society. Pipe used in grommets shall be standard weight (Schedule 40) steel pipe. The entire assemblies shall be hot dip galvanized after fabrication in accordance with ASTM A-123. Bolts, nuts, and washers shall be hot dip galvanized conforming to ASTM A-307.
- L. Structural Steel – Grommet assemblies and steel rubbing angles shall be fabricated as shown on the plans and the structural steel used shall conform to ASTM A36. All welding shall conform to the requirements of the American Welding Society. Pipe used in grommets shall be standard weight (Schedule 40) steel pipe. The entire assembly shall be hot dip galvanized after fabrication in accordance with ASTM A-123.
- M. Cold Galvanizing Compound - Cold galvanizing compound shall be “ZRC Cold Galvanizing Compound” as manufactured by ZRC Worldwide or approved equal.
- N. Tires – The existing rubber tires from the fender system shall be reused in the newly repaired fender system after being inspected and approved by the Construction Engineer and Harbor Agent. **New rubber tires must be between 5 to 7.5 feet in diameter and between 2 to 3 feet in width.** Newly provided worn tires may be accepted for use in place of new tires and shall be individually inspected and approved by the Construction Engineer or Harbor Agent prior to use in the newly repaired fender system. It is the Contractor's responsibility to provide enough satisfactory worn or brand-new tires to completely repair the fender system, with any additional tire fenders placed in a location deemed appropriate by the Construction Engineer and Harbor Agent for storage and future use.

FENDER REPAIRS

AT

PIER 3, HILO HARBOR, HAWAII

S50208

PROPOSAL SCHEDULE

Item No.	Item Description	Approx. Quantity (a)	Unit	Unit Price (b)	Amount Bid (a x b)
1	Repair Fender System	1 Job	Lump Sum	Lump Sum	\$ _____
2	Concrete Haunch Spall Repair	150	SF	\$ _____	\$ _____
3	Concrete Haunch Full Repair	6	Each	\$ _____	\$ _____
4	Replacement Tire Bumpers	10	Each	\$ _____	\$ _____
5	New Tire Bumper Hardware	115	Each	\$ _____	\$ _____
TOTAL AMOUNT FOR COMPARISON OF BIDS.....					\$ _____

NOTES:

Bids shall include all Federal, State, County and other applicable taxes and fees.

The TOTAL AMOUNT FOR COMPARISON OF BIDS will be used to determine the lowest responsible bidder.

Bidders shall complete all unit prices and amounts. Failure to do so may be ground for rejection of bid.

If a discrepancy occurs between unit bid price and the bid price, the unit bid price shall govern.

The bidder shall submit the proposal in HIePRO. The proposal shall be UPLOADED to HIePRO prior to the bid opening date and time. Proposals received after said due date and time shall not be considered. Original (wet ink) proposal documents are not required to be submitted. The award will be made based on proposals uploaded in HIePRO. Any and all other additional

documents explicitly designated and labeled as CONFIDENTIAL or PROPRIETARY shall be UPLOADED SEPARATELY to HIePRO.

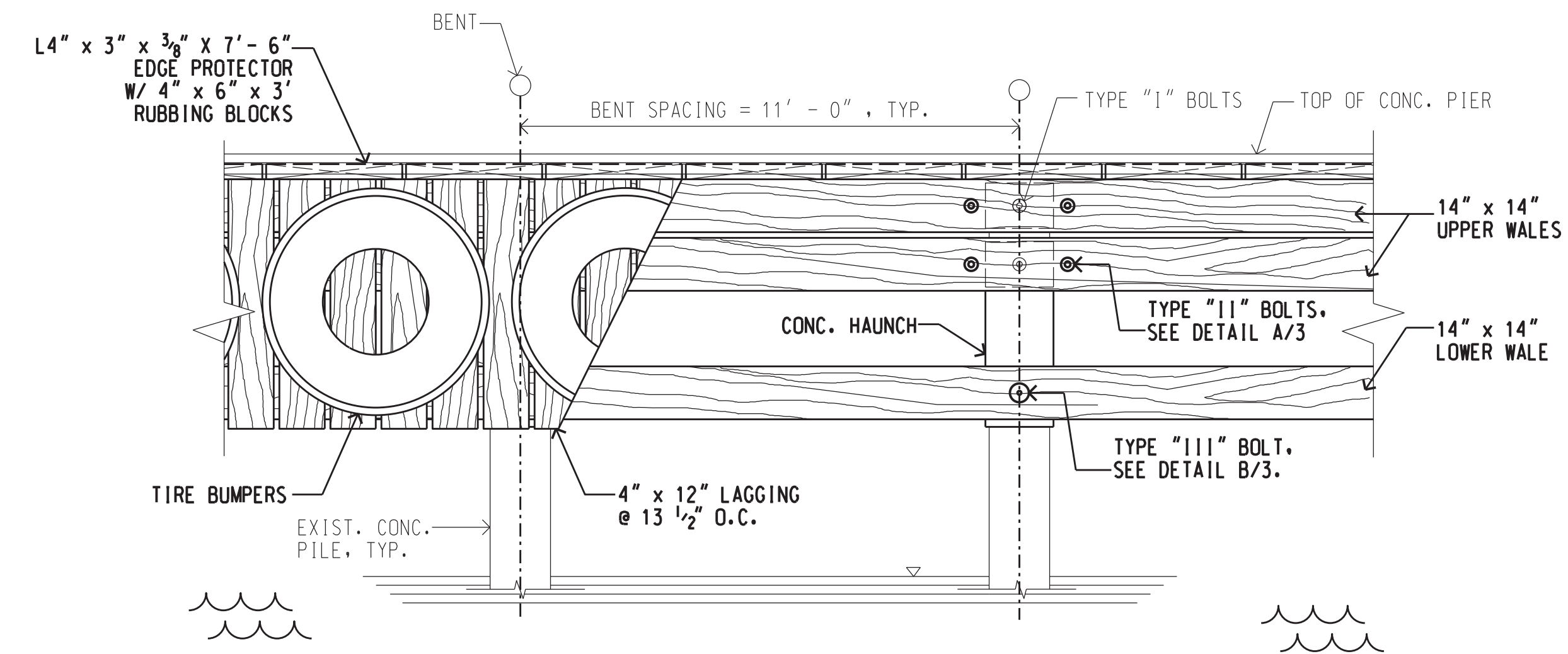
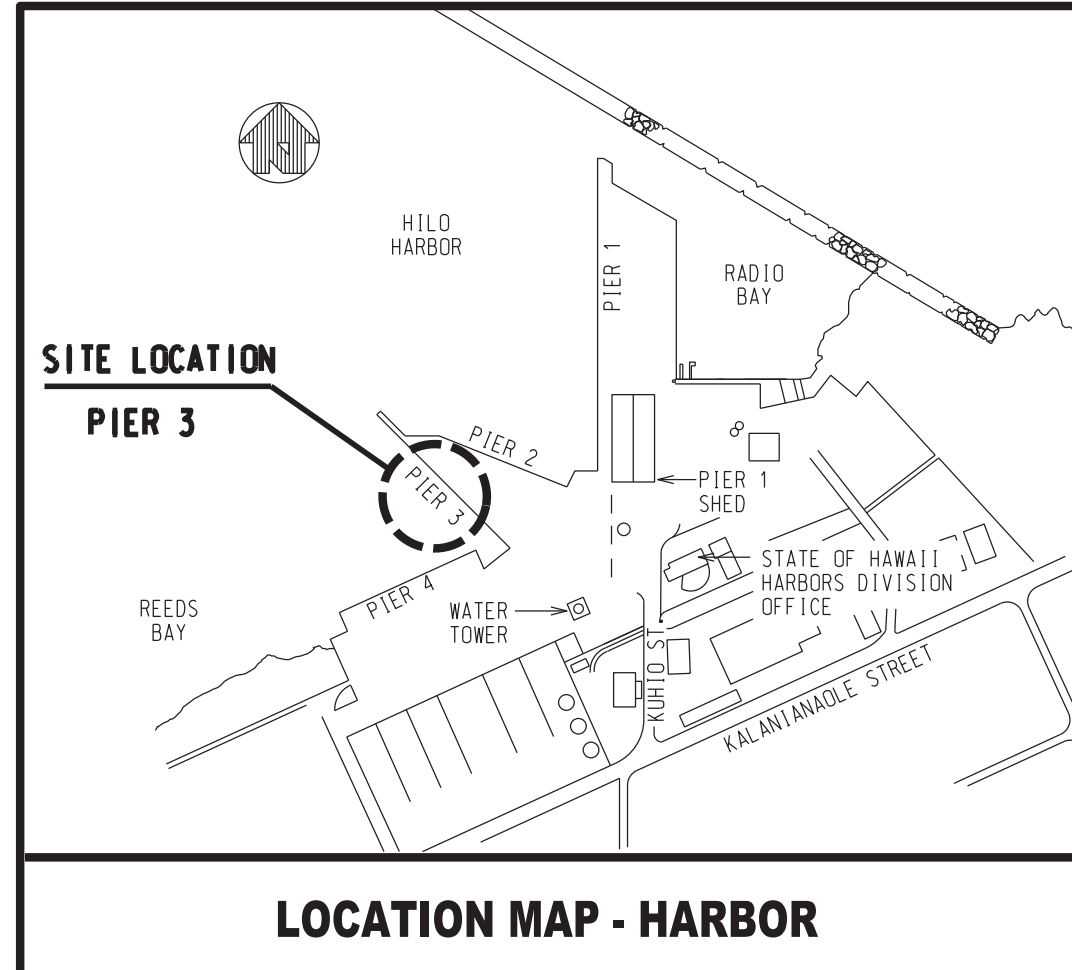
Bids will be evaluated on the basis of the TOTAL AMOUNT FOR COMPARISON OF BIDS. If the lowest TOTAL AMOUNT FOR COMPARISON OF BIDS is less than, or approximately equal to the funds available for this project, an award will be made to the lowest responsible bidder.

If the lowest TOTAL AMOUNT FOR COMPARISON OF BIDS exceeds the funds available for this project, the State reserves the right to negotiate with the lowest responsible bidder as permitted under Hawaii Revised Statutes, Section 103D-302, as amended, to reduce the scope of work and award a contract.

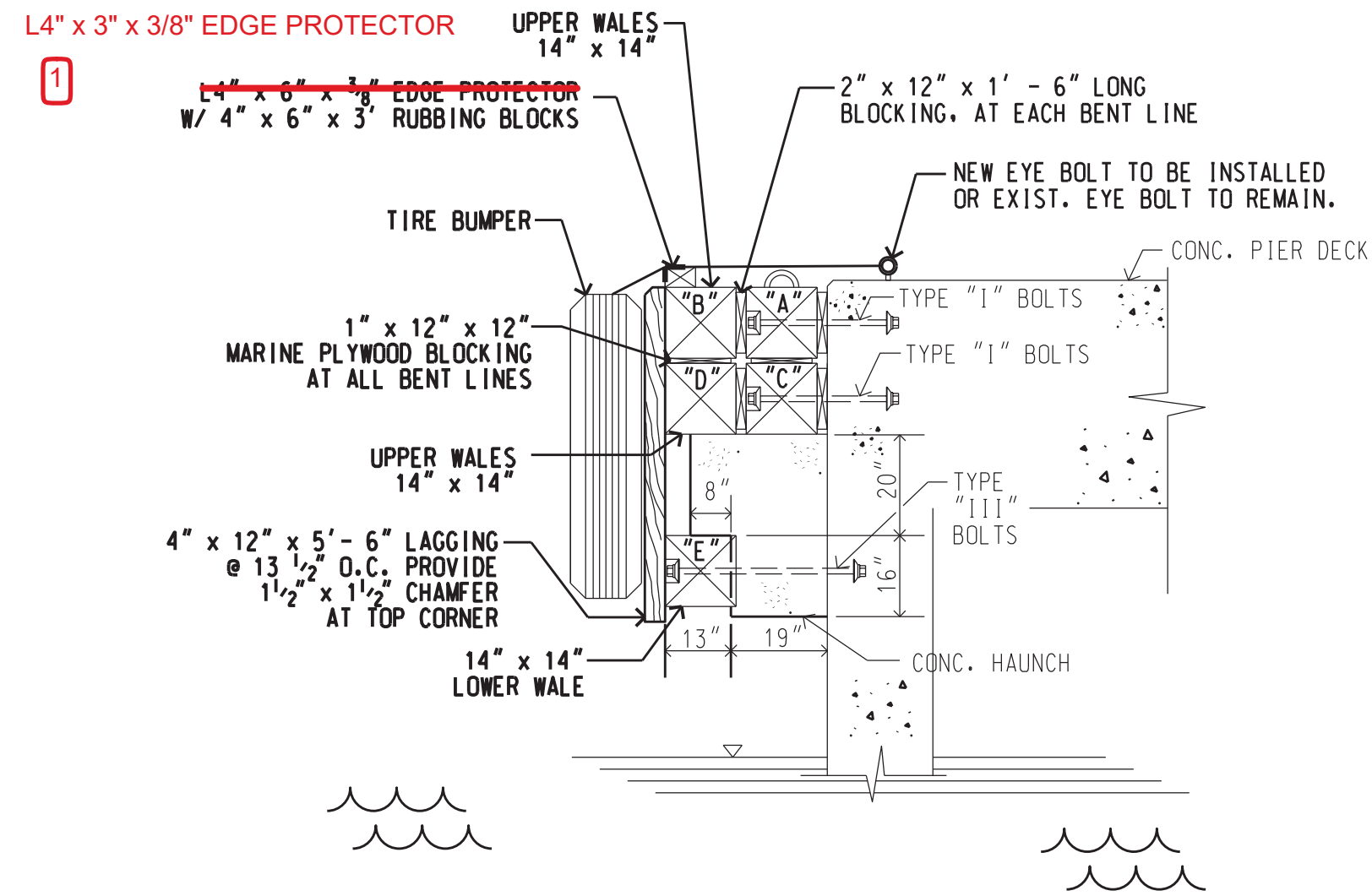
Submission of Proposal is a warranty that the bidder has made an examination of the project site and is fully aware of all conditions to be encountered in performing the work and the requirements of the plans and specifications.

No additional compensation will be paid by the State for losses, including overhead and profit, resulting from reduced scope of work.

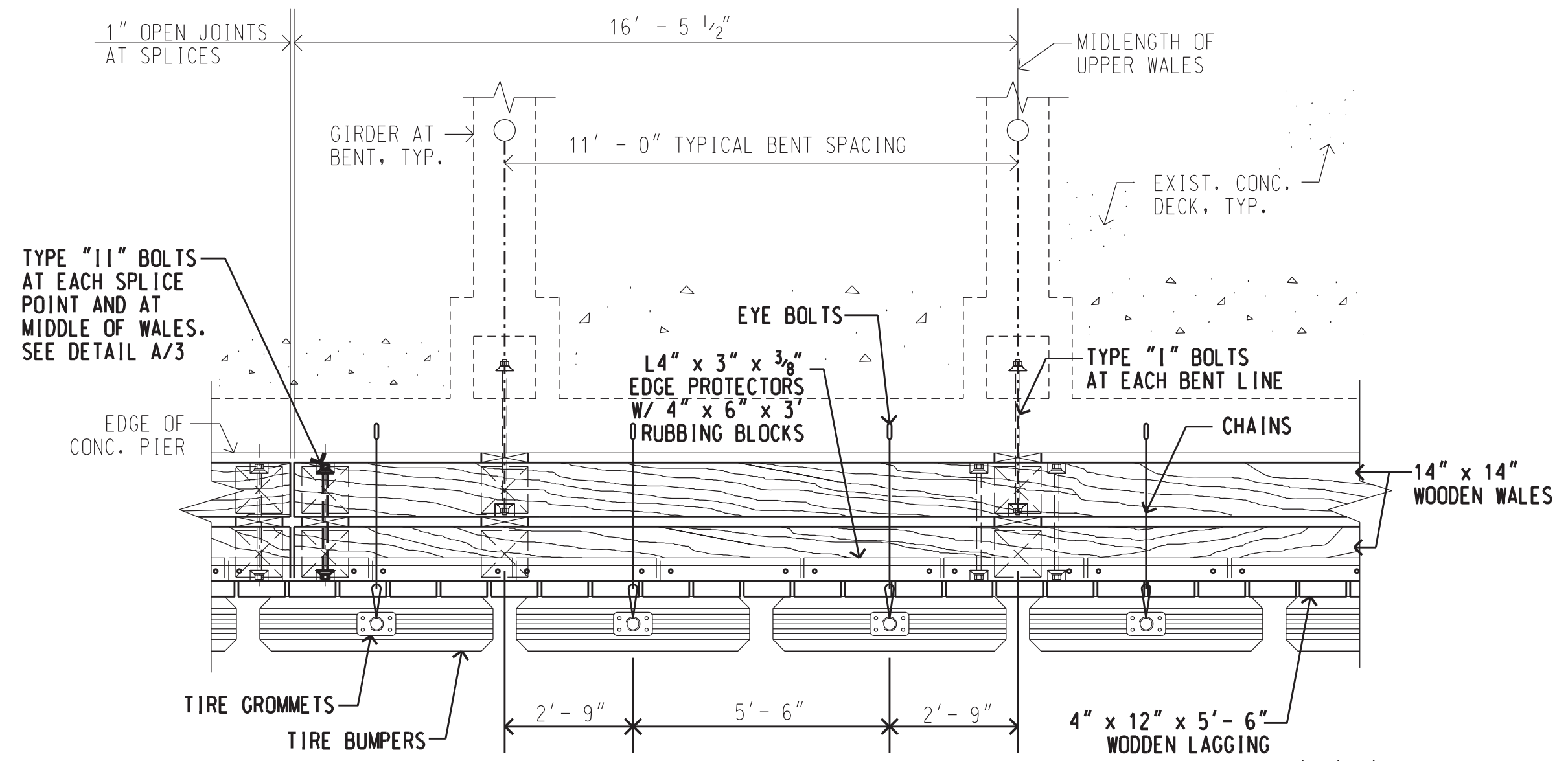
Contract time shall remain the same whether or not the overall scope of work is decreased.



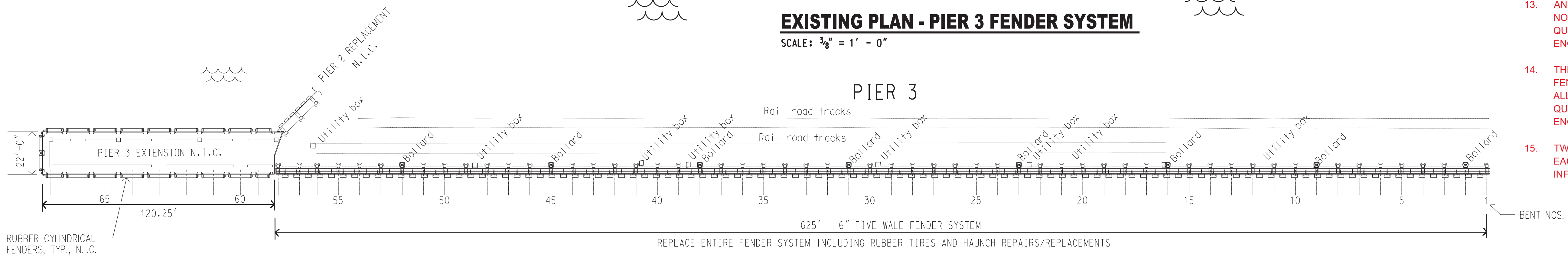
EXISTING ELEVATION - PIER 3 FENDER SYSTEM
SCALE: 3/8" = 1' - 0"



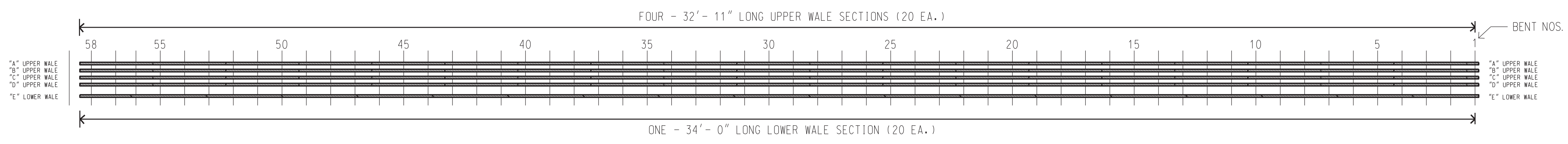
EXISTING SECTION - PIER 3 FENDER SYSTEM
SCALE: 3/8" = 1' - 0"



EXISTING PLAN - PIER 3 FENDER SYSTEM
SCALE: 3/8" = 1' - 0"



PLAN - PIER 3
SCALE: 1" = 30' - 0"

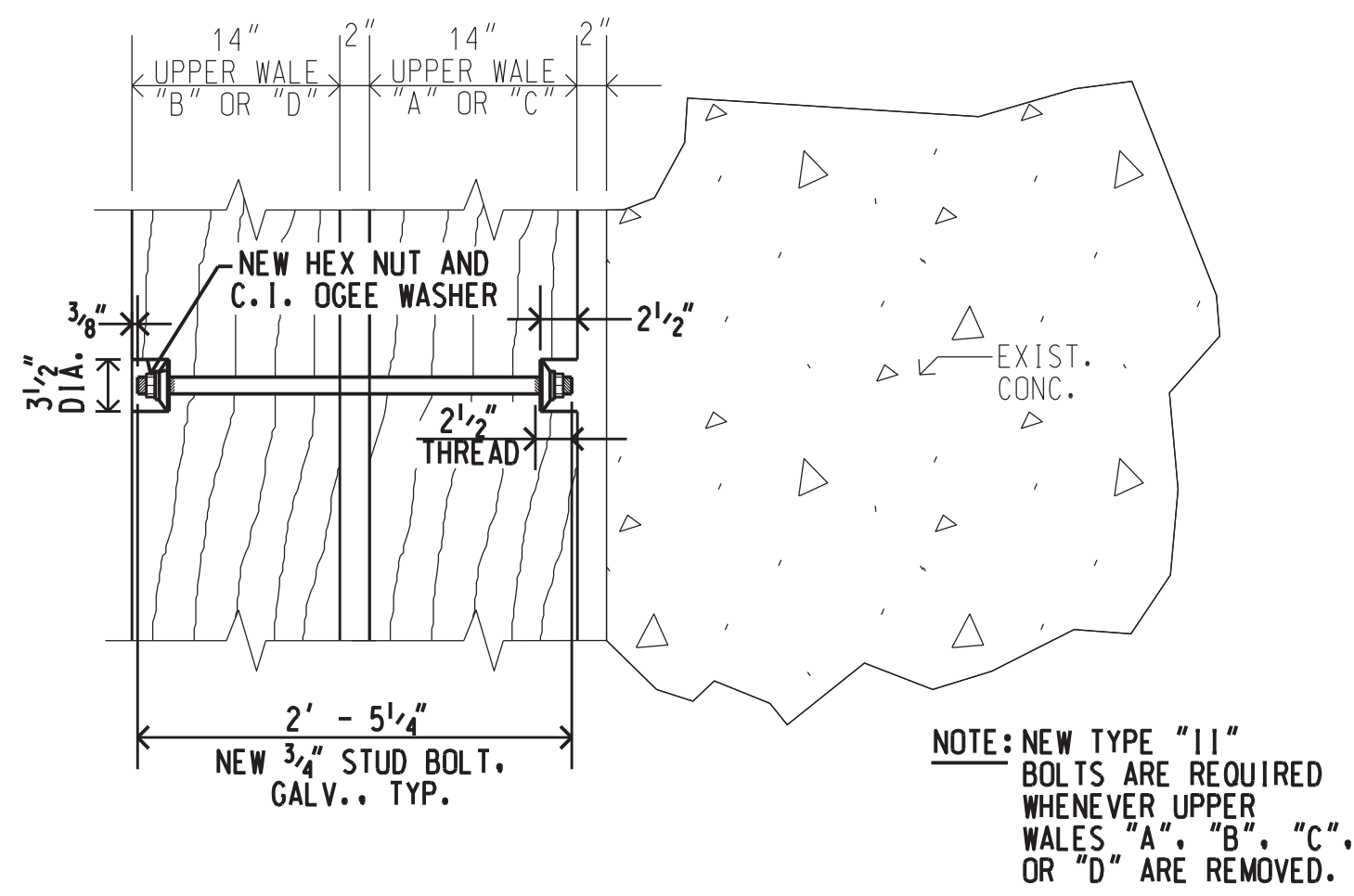


PIER 3 - FENDER WALE REPLACEMENT PLAN
SCALE: 1" = 30' - 0"

GENERAL NOTES

- CONTRACTOR SHALL VERIFY CONDITIONS IN FIELD PRIOR TO BIDDING. EXISTING CONDITIONS ARE BASED ON THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL MAKE NO CLAIM FOR EXTRA COMPENSATION SHOULD ACTUAL EXISTING CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL VERIFY CONDITIONS AND DIMENSIONS IN THE FIELD PRIOR TO ORDERING MATERIALS. SEE THE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- CONTRACTOR SHALL EXERCISE CAUTION TO PREVENT DAMAGE TO EXISTING STRUCTURES, UTILITIES, AND EQUIPMENT IN THE AREA.
- CONTRACTOR SHALL COORDINATE ITS WORK WITH THE HARBORS DIVISION HAWAII DISTRICT MANAGER AND THE CONSTRUCTION ENGINEER SO AS TO MINIMIZE INTERFERENCE WITH HARBOR OPERATIONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO WORK WHEN A PASSANGER CRUISE VESSEL OR FUEL BARGE ARE BERTHED AT THE PIER. THE EXACT SCHEDULING OF THE WORK AND RESTRICTIONS ON THE CONTRACTOR'S ACTIVITIES WILL BE ESTABLISHED AT THE PRE-CONSTRUCTION MEETING.
- THE CONTRACTOR SHALL COMPLY WITH THE CLEAN WATER ACT AND THE STATE OF HAWAII DEPARTMENT OF TRANSPORTATION HARBORS DIVISION STORMWATER MANAGEMENT PROGRAM. NO POLLUTANTS ARE ALLOWED TO BE DISCHARGED DIRECTLY OR INDIRECTLY THROUGH THE HARBORS SMALL MS4 OR OTHER POTENTIAL PATHWAYS INTO ADJACENT HARBOR WATERS.
- THE CONTRACTOR SHALL SUBMIT A SITE-SPECIFIC BEST MANAGEMENT PRACTICE (BMP) PLAN TO HARBORS CONSTRUCTION ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO THE START OF ANY CONSTRUCTION WORK. THE SITE-SPECIFIC BMP PLAN SHALL COMPLY WITH THE TEMPORARY WATER POLLUTION, DUST, AND EROSION CONTROL ARTICLE IN THE SPECIFICATIONS AND PROPOSAL.
- IN CASE OF SPILL, LEAK, OR OTHER RELEASE CONTAINING A HAZARDOUS SUBSTANCE OR OIL, THE CONTRACTOR SHALL NOTIFY APPROPRIATE FACILITY PERSONNEL, EMERGENCY RESPONSE AGENCIES, AND REGULATORY AGENCIES FOLLOWING NOTIFICATION PROCEDURES, AND SHALL NOTIFY HARBORS DIVISION ENVIRONMENTAL HOTLINE (808-587-1962) AND DESIGNATED PROJECT ENGINEER IMMEDIATELY (I.E. WITHIN 24-HOURS). SUCH CONTACT INFORMATION MUST BE SHOWN IN LOCATIONS THAT ARE READILY ACCESSIBLE AND AVAILABLE.
- ALL STRUCTURES AND PORTIONS OF STRUCTURES SHOWN ON THE PLANS ARE EXISTING UNLESS SPECIFICALLY NOTED. CLEAR WITH HAWAII ONE CALL CENTER (HOCC) BEFORE EXCAVATION (811) OR (866-427-7287).
- 6 CONC. HAUNCH FULL REPLACEMENTS AND 150 SF OF CONC. HAUNCH SPALL REPAIRS ARE INCLUDED IN THE PROPOSAL SCHEDULE FOR BIDDING PURPOSES. THE EXACT LOCATIONS OF THE CONC. HAUNCH REPAIRS AND REPLACEMENTS WILL BE DETERMINED IN THE FIELD AND THE QUANTITIES ARE SUBJECT TO CHANGE BY INSTRUCTION FROM THE CONSTRUCTION ENGINEER.
- ALL EXISTING TIRE FENDERS ARE TO BE INSPECTED AND APPROVED FOR RE-INSTALLATION BY THE CONSTRUCTION ENGINEER AND HARBOR AGENT. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE QUANTITIES OF VARIOUS SIZED TIRE FENDERS WITH THE CONSTRUCTION ENGINEER AND HARBOR AGENT BEFORE BIDDING AND CONSTRUCTION BEGINS.
- SEE THE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- 10 RUBBER TIRE BUMPERS REPLACEMENTS ARE INCLUDED IN THE PROPOSAL SCHEDULE FOR BIDDING PURPOSES. THE EXACT LOCATIONS OF THE RUBBER TIRE BUMPER REPLACEMENTS WILL BE DETERMINED IN THE FIELD AND THE QUANTITIES ARE SUBJECT TO CHANGE BY INSTRUCTION FROM THE CONSTRUCTION ENGINEER.
- AN ESTIMATED 20 NEW EYE BOLTS ARE TO BE INSTALLED ROUGHLY NEAR THE BENT NOS. 10 THROUGH 0. THE EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD AND THE QUANTITIES ARE SUBJECT TO CHANGE BY INSTRUCTION FROM THE CONSTRUCTION ENGINEER. SEE DETAILS AND SPECIFICATIONS FOR MORE INFORMATION.
- THERE ARE AN ESTIMATED 115 RUBBER TIRE BUMPERS IN TOTAL TO COMPLETE THE FENDER SYSTEM. 13 LF OF CHAIN IS USED FOR EACH SINGLE RUBBER TIRE BUMPER. ALL OF THE CHAIN IS TO BE REPLACED AND WILL BE DETERMINED IN THE FIELD WITH QUANTITIES BEING SUBJECT TO CHANGE BY INSTRUCTION FROM THE CONSTRUCTION ENGINEER... SEE DETAILS AND SPECIFICATIONS FOR MORE INFORMATION..
- TWO (2) NEW TIRE GROMMETS AND TWO (2) NEW SHACKLES WILL BE INSTALLED ON EACH RUBBER TIRE BUMPER. SEE DETAILS AND SPECIFICATIONS FOR MORE INFORMATION.

REV	DATE	DESCRIPTION	BY	APPROVED
1	5/25/23	ADDENDUM NO.1	JM	AF
STATE OF HAWAII DEPARTMENT OF TRANSPORTATION HARBORS DIVISION				
JOB TITLE FENDER REPAIRS AT PIER 3 HILO HARBOR, HILO, HAWAII				
PIER 3 - PLAN, ELEVATION, SECTION, AND NOTES				
DESIGNED BY:	JM	JOB NUMBER		SHEET
DRAWN BY:	JM	S50208		2
CHECKED BY:	AF			
DATE:	APR 2023			2 OF 5 SHTS.
SCALE:	AS SHOWN			

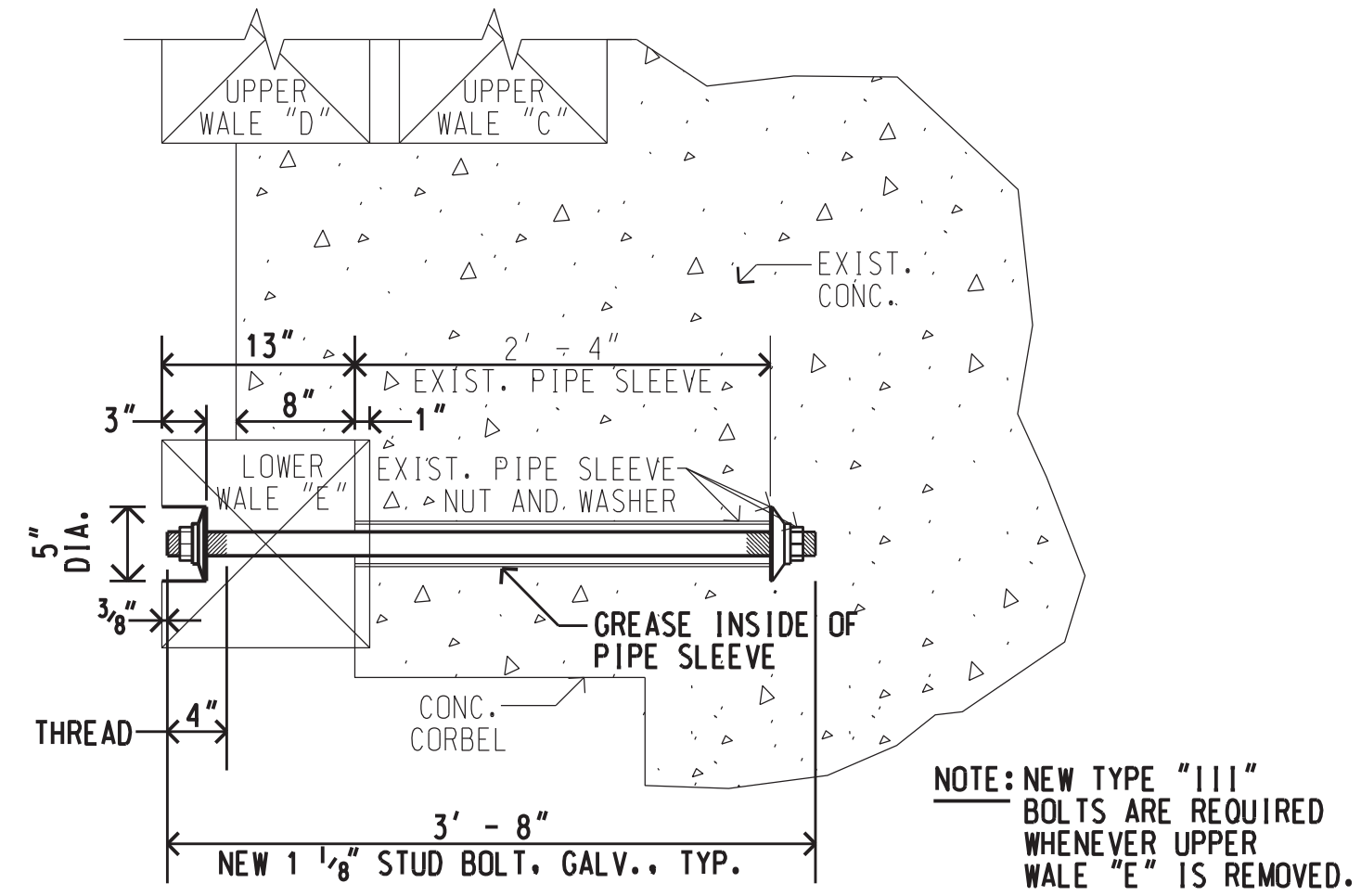


PLAN - TYPE "II" BOLTS

SCALE: 1 1/2" = 1' - 0"

A
3

NOTE: NEW TYPE "II" BOLTS ARE REQUIRED WHENEVER UPPER WALES "A", "B", "C", OR "D" ARE REMOVED.

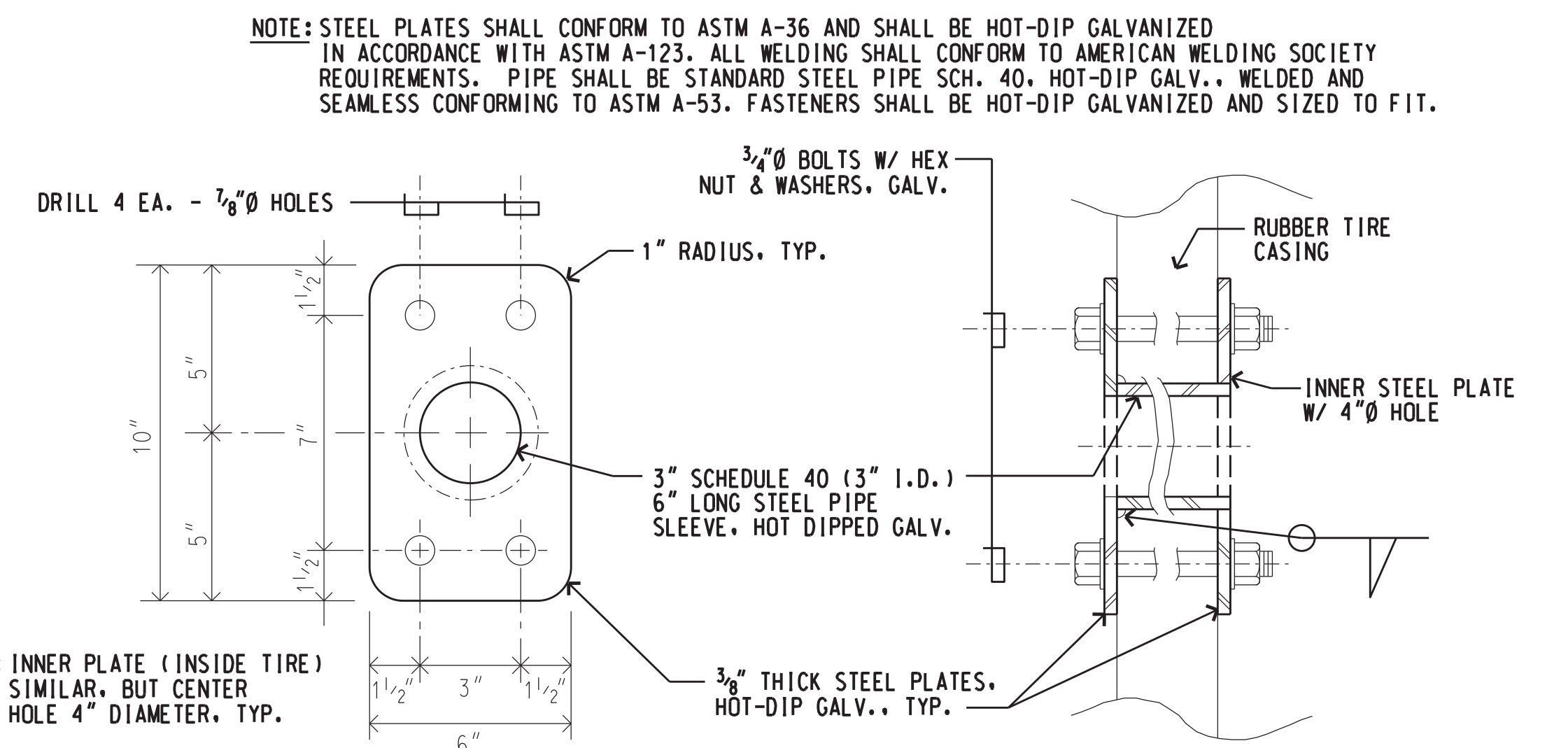


SECTION - TYPE "III" BOLTS

SCALE: 1 1/2" = 1' - 0"

D
3

NOTE: NEW TYPE "III" BOLTS ARE REQUIRED WHENEVER UPPER WALE "E" IS REMOVED.



NOTE: INNER PLATE (INSIDE TIRE) SIMILAR, BUT CENTER HOLE 4" DIAMETER, TYP.

NOTE: HOT-DIP GALV. AFTER FABRICATION. QUANTITY TO BE DETERMINED IN FIELD.

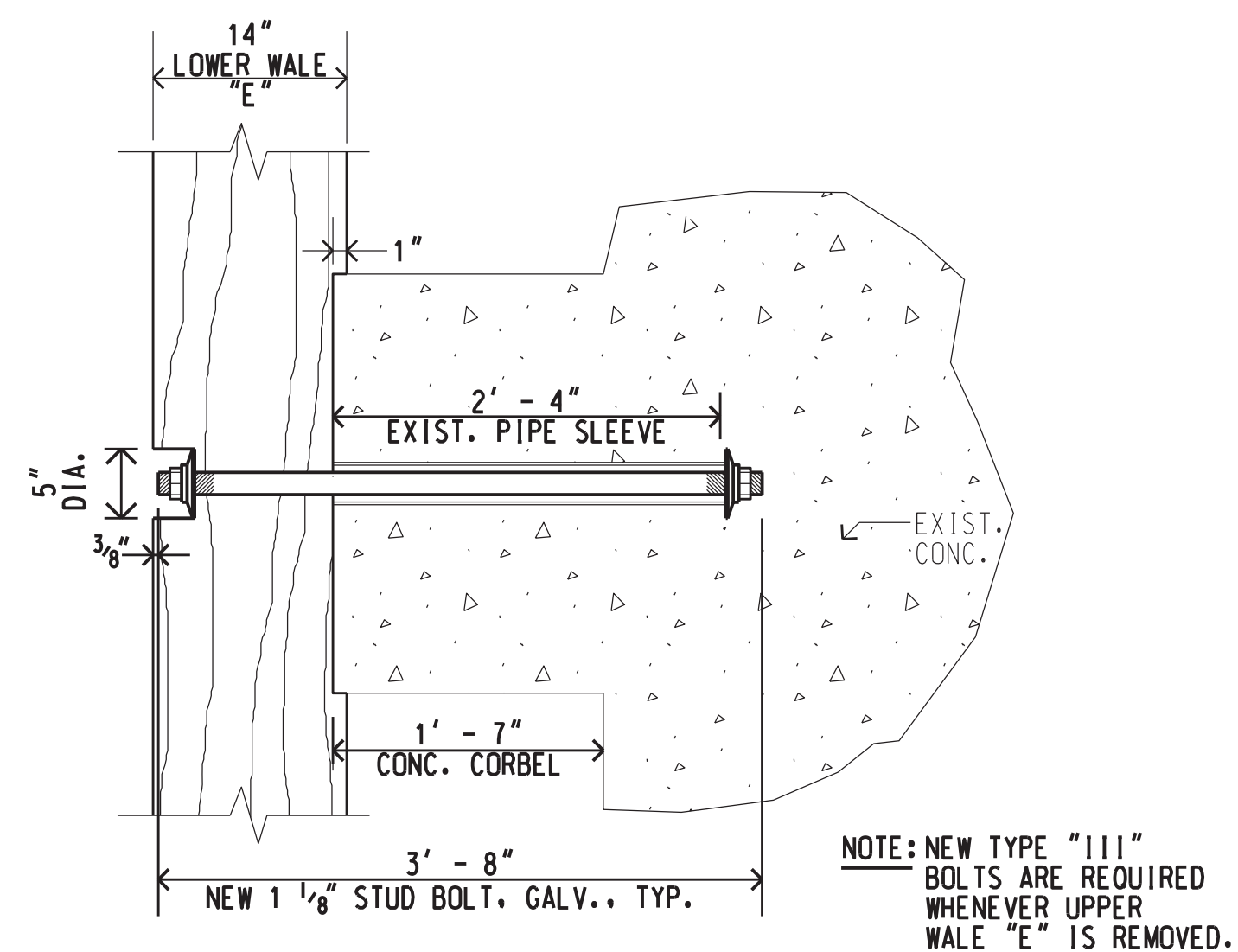
PLAN

SECTION

TIRE GROMMET DETAIL

SCALE: 3" = 1' - 0"

F
3

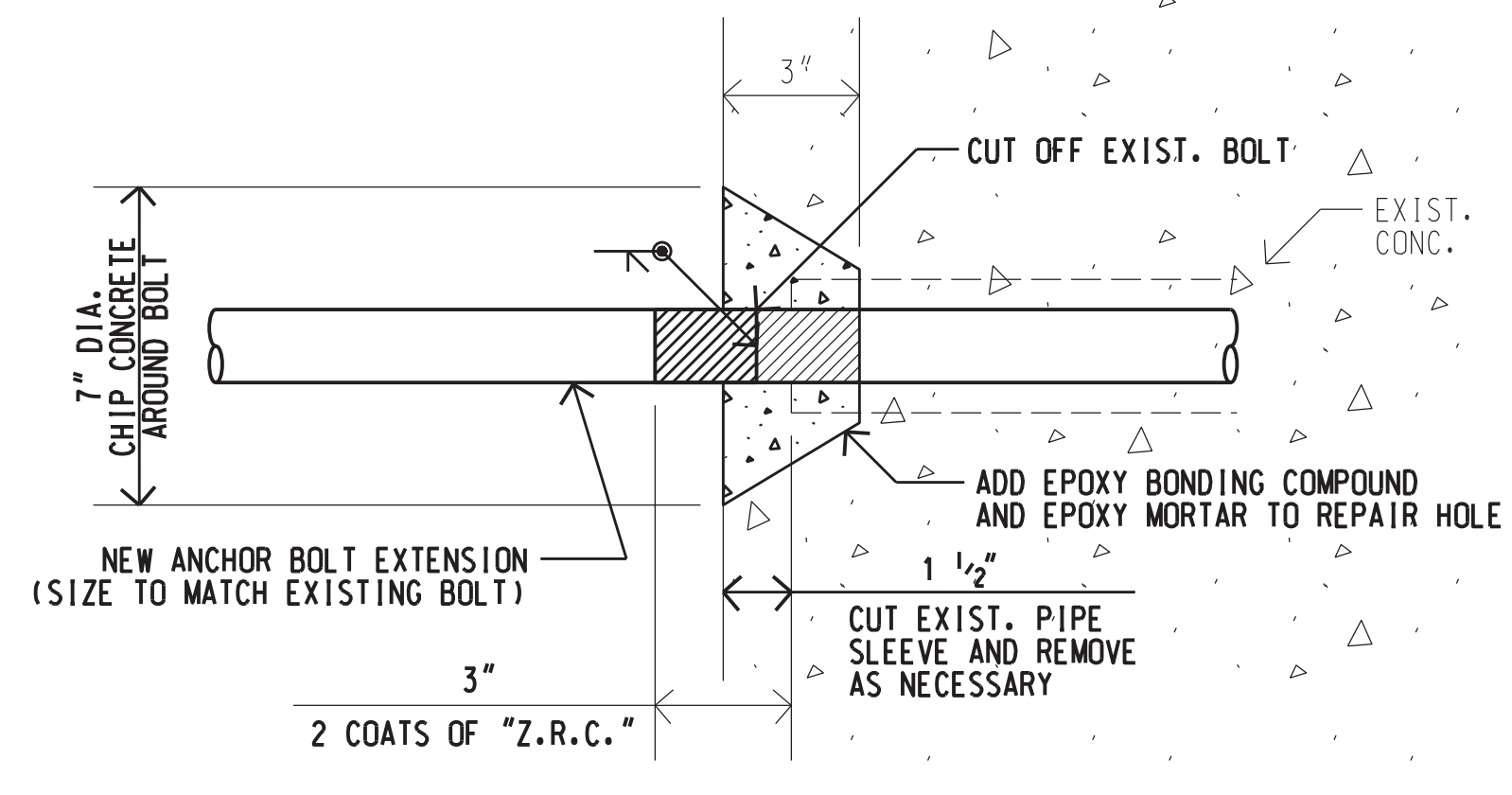


PLAN - TYPE "III" BOLTS

SCALE: 1 1/2" = 1' - 0"

B
3

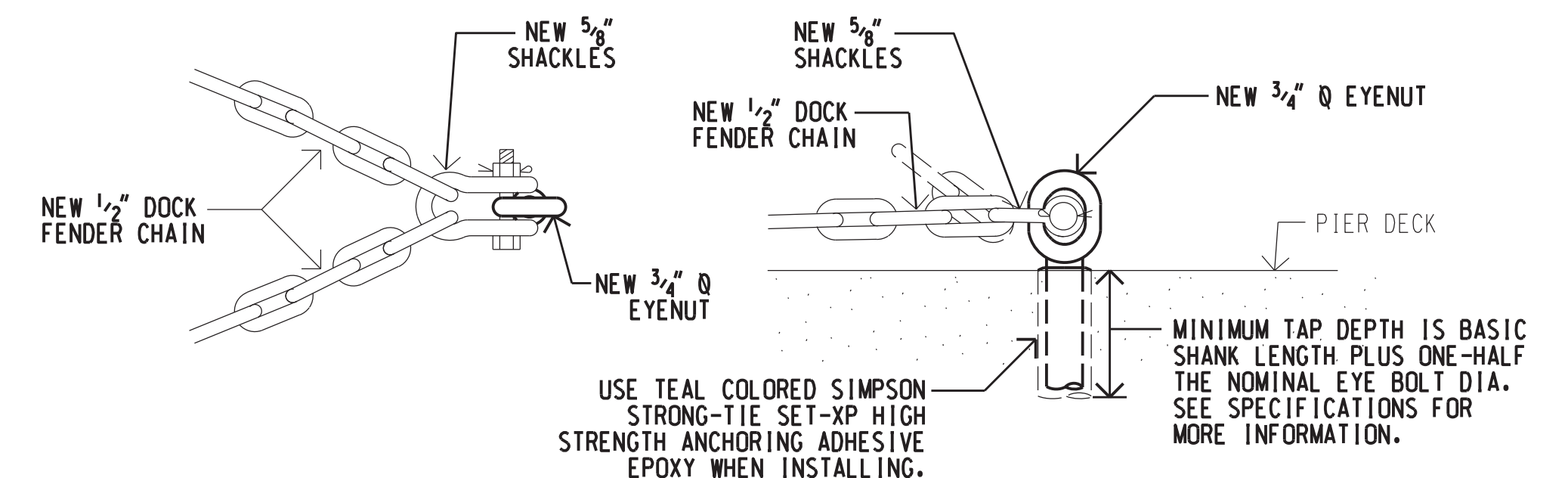
NOTE: NEW TYPE "III" BOLTS ARE REQUIRED WHENEVER UPPER WALE "E" IS REMOVED.



DETAIL - TYPE "III" BOLT EXTENSION

SCALE: 3" = 1' - 0"

E
3



PLAN

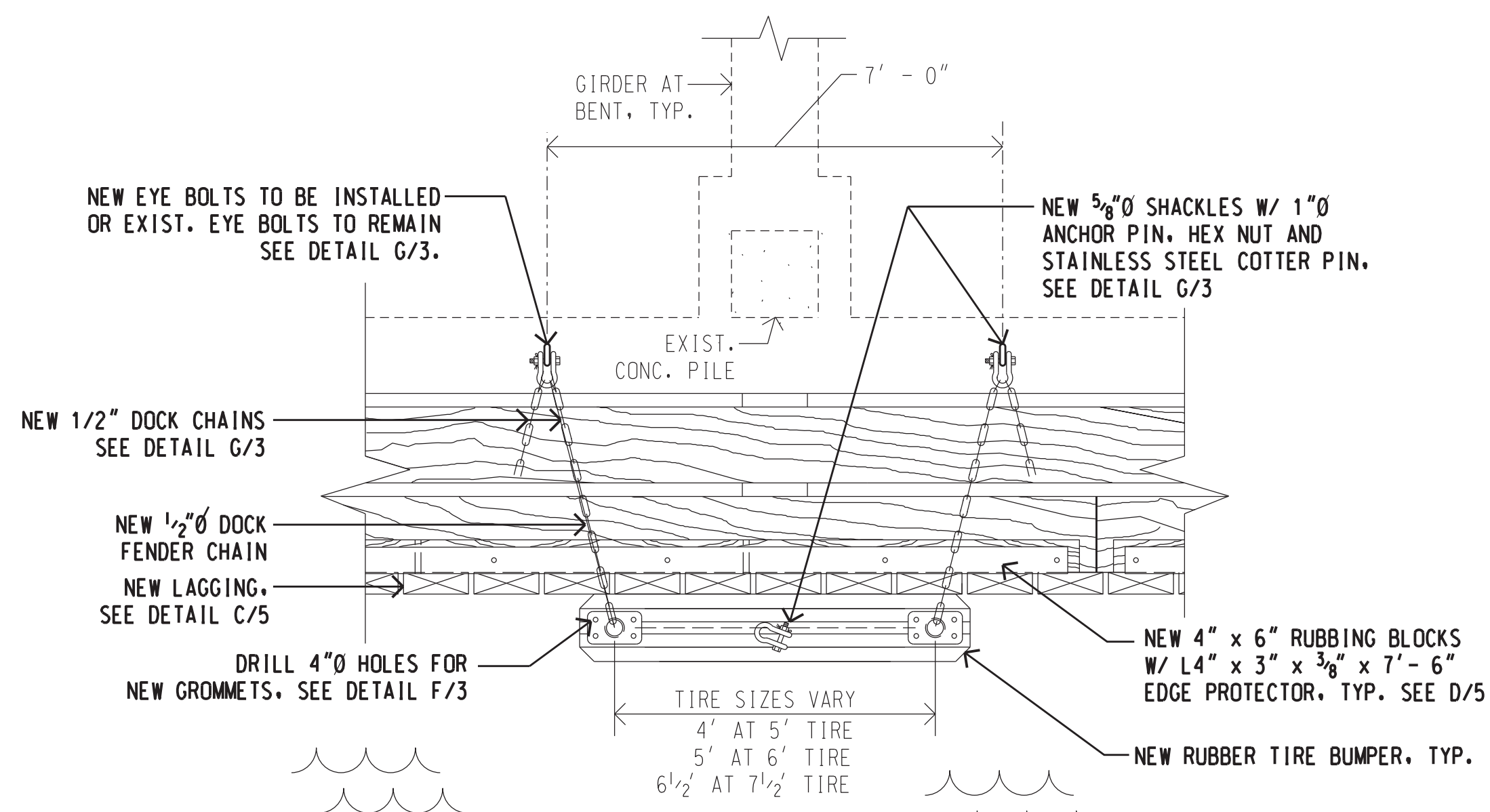
SECTION

EYENUT ASSEMBLY DETAIL

SCALE: 1 1/2" = 1' - 0"

G
3

REVISED EYENUT ASSEMBLY DETAIL G/3

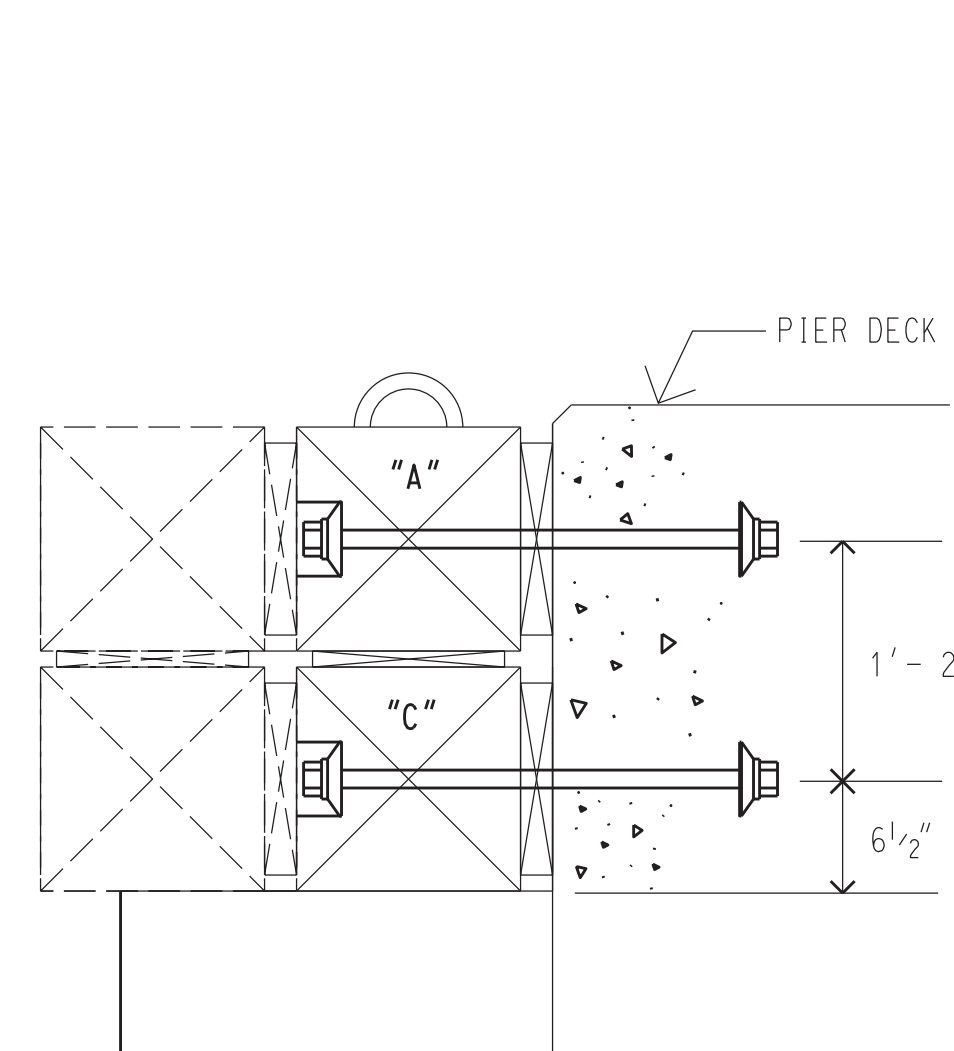


NEW TIRE BUMPER ASSEMBLY DETAIL AT EYE-BOLTS

SCALE: 1/2" = 1' - 0"

REVISED NEW TIRE BUMPER ASSEMBLY DETAIL AT EYE-BOLTS DETAIL C/3

C
3

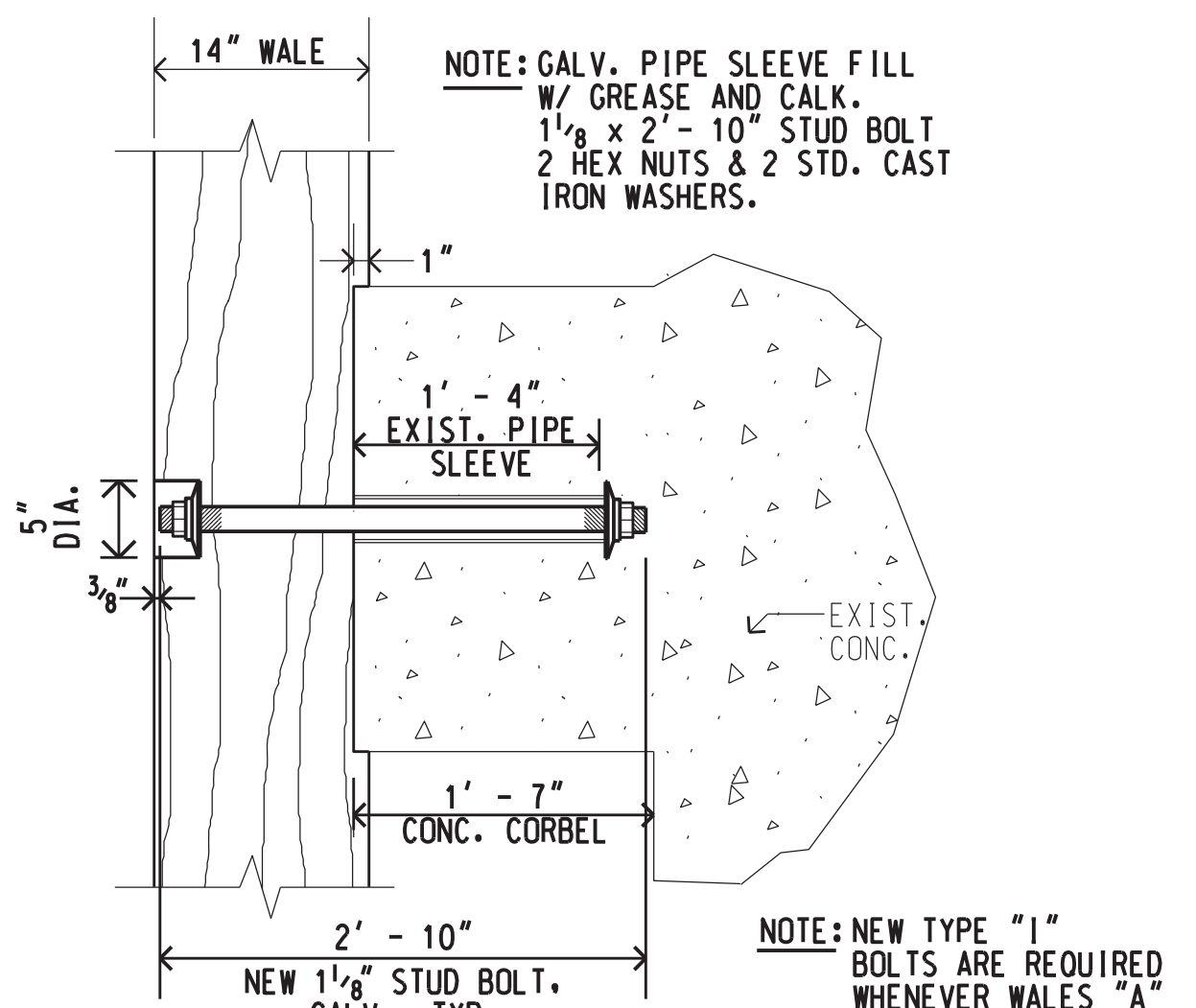


SECTION

TYPE "I" BOLTS

SCALE: 1 1/2" = 1' - 0"

NEW TYPE "I" BOLTS DETAIL H/3



PLAN

REV	DATE	DESCRIPTION	BY	APPROVED
1	5/25/23	ADDENDUM NO. 1	JM	AF
STATE OF HAWAII DEPARTMENT OF TRANSPORTATION HARBORS DIVISION				
JOB TITLE FENDER REPAIRS AT PIER 3 HILO HARBOR, HILO, HAWAII				
PIER 3 - REPAIR DETAILS				
DESIGNED BY:	JM	JOB NUMBER		SHEET
DRAWN BY:	JM	S50208		3
CHECKED BY:	AF			
DATE:	APR 2023	SCALE:		AS SHOWN
				3 of 5 SHTS.

MINUTES TO PRE-BID MEETING

Project: Fender Repairs at Pier 3, Hilo Harbor, Hawaii - Job S50208

Date: Friday, May 19, 2023

Time: 9:00 a.m.

Place: Meeting was held virtually on Microsoft Teams

IN ATTENDANCE

Name:	Company/Organization:	Email:
James Miller	Department of Transportation, Harbors	James.B.Miller@hawaii.gov
Lizi Olson	Jas. W. Glover, Ltd.	LiziO@gloverltd.com
Jordan Enos	Isemoto Contracting Co., Ltd.	JordanE@isemotocontracting.com
Ryan Arfman	Sea Engineering, Inc.	rarfman@seaengineering.com
Russell Luke	Sea Engineering, Inc.	rluke@seaengineering.com

ITEMS OF DISCUSSION

- A. The Department of Transportation, Harbors (DOTH), Project Manager, Mr. James Miller, introduced himself at 9:01 a.m. and gave a brief review of the Notice to Bidders (NTB) meeting information stating that anything said at the pre-bid meeting is for clarification purposes. The meeting minutes and questions for this pre-bid meeting, along with any changes to the bid documents, will be made by addendum and posted in HIePRO, and included on page P-4 of the Proposal.
- B. There is a question submittal tab in HIePRO that may be utilized by the contactors/vendors if they have any further questions. The date and time deadline for posting questions online through HIePRO is on May 24, 2023, at 2:00 p.m. Verbal requests for information (RFI) and questions received after the deadline will not be addressed.
- C. DOTD reviewed the project scope, which consists of the following:
 - 1. Removal of complete fender system.
 - 2. Repair and/or repairing concrete haunches.
 - 3. Replacement of complete fender system including tire fenders, lagging, wales, blocking, bolts, grommets, chains, hardware, and any other appurtenances.
- D. Bid Open Date: Friday, June 9, 2023, at 2:00 p.m.
- E. Project Schedule: Completion time is 300 calendar days after Notice to Proceed issuance
- F. Mr. Miller addressed any questions submitted through HIePRO.

G. Bidders were invited to ask questions:

1. Q: Is there currently a phasing plan?

A: There is no established phasing plan currently. Contractors are to schedule their work with the Construction Engineer so that they do not interfere with harbor operations along Pier 3.

2. Q: Is there a limit to the works proximity to the Pier 3 harbor operations?

A: The work to be done is applied directly to the Pier 3 face, so any and all Pier 3 adjacent operations will be affected by this project. Again, contractors are to work with the Construction Engineer and harbor operations with scheduling appropriate work timelines.

3. Q: Please clarify HiePRO question regarding a tire replacement quantity.

A: The addendum is going to include changes to the Specifications which gives a best estimate to the tire replacement quantity. The majority of existing tires will be reinstalled with new grommet holes, grommets, chains and shackles.

H. The following reminder was given:

1. Reviewed NTB information regarding bid document questions and RFI shall be submitted via HiePRO no later than May 24, 2023, at 2:00 p.m. Verbal RFI and questions received after the deadline will not be addressed.

I. The meeting concluded at 9:15 a.m.